

Augusta Road Master Plan

Phase I: Augusta Road at Mauldin Road



Mahan Rykiel Associates, Inc.
Community Design Solutions

August 15, 2011

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ACKNOWLEDGEMENTS

We would like to thank the following participants for their time, dedication and support throughout the planning process.

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I: INTRODUCTION

In May 2010, City of Greenville staff began working with residents, business owners and property owners to develop a master plan for the southeastern portion of Augusta Street, or Augusta Road, as it is referred to locally. The study area is generally bounded by Blythe Academy/Riverside Drive to the north and Interstate 85 (I-85) to the south. A variety of uses and development patterns exist within the study area, including office and institutional uses, a wide range of commercial uses, as well as single family and multi-family residential. The study area is also unique in that it includes properties located within the incorporated limits of the City of Greenville, as well as properties located in Greenville County.

The area is one of the most visible gateways into the City of Greenville, connecting I-85 directly to downtown Greenville. Within the city limits, the Augusta Street area is surrounded by well-established residential neighborhoods. These neighborhoods are home to several schools, churches, parks, a community center and a public library. Additionally, Greenville Country Club and Riverside Golf Course are located within the neighborhood just east of the study area. However, over the years, commercial properties in the area have deteriorated and businesses have declined. Traffic, safety and crime have become concerns among area stakeholders who envision a livable, walkable and economically viable commercial district that will complement and support the needs of the surrounding neighborhoods.

The Augusta Road Master Plan represents the first of many steps in an ongoing, long-range revitalization effort. The plan provides recommendations for strategic investments - both public and private - that will serve as catalysts for further economic growth. The primary goal of this effort is to ensure future development will enhance and protect the character of the commercial district and the unique and diverse neighborhoods which surround the Augusta Road study area.

Project Partners

City of Greenville, SC

Website: www.greenvillesc.gov

Greenville County, SC

Website: www.greenvillecounty.org

Augusta Road Business Association: The Augusta Road Business Association (ARBA) is a coalition of businesses established to work together for the betterment of the Augusta Road corridor. ARBA members are actively involved in local economic development efforts, and have partnered with the City of Greenville to facilitate a long-term improvement plan for the study area. ARBA currently has over 70 members. Membership is open to any business having an interest in Augusta Road. Website: www.onlyonaugusta.com

Preserve Augusta Road Gateway, Inc. is a non-profit organization formed by Augusta Road residents and business owners with one purpose: to encourage the betterment of the Augusta Road “gateway,” including the unique and diverse neighborhood and commercial areas which surround Augusta Road, primarily from Mauldin Road to Augusta Place.

Goals and Objectives

Goal 1: Maintain and strengthen surrounding neighborhoods

- Work with City’s Community Development Department to develop a master plan for the Pleasant Valley neighborhood.
- Increase code enforcement efforts to address issues related to absentee property owners or landlords, neglected or poorly maintained property, and abandoned/vacant structures.
- Work with local development community to increase home ownership opportunities.
- Develop capacity of neighborhood associations and residents to participate in revitalization efforts.
- Encourage collaboration and partnerships with nearby schools, churches and other community agencies to contribute to revitalization of the area.

Goal 2: Enhance and beautify “gateway” into the City

- Develop a streetscape plan to enhance the character of the area.
- Survey existing tree canopy and identify locations for tree plantings.

- Continue to promote City's Facade Improvement Program, which provides financial assistance to commercial property owners and businesses who desire to improve the appearance of their buildings.
- Create a sense of place by encouraging quality architecture and design.
- Address property maintenance concerns along the corridor.

Goal 3: Achieve balance through strategic growth

- Encourage economic growth while respecting existing character of the area.
- Assess effectiveness of existing regulatory tools and policies.
- Address inconsistencies between stake-holders' vision for the area and existing zoning and land-use classifications.
- Evaluate existing infrastructure capacity and identify needed improvements.
- Increase communication between neighborhood and City/County to ensure residents are more aware of the development process.

Goal 4: Provide a safe environment

- Improve coordination between City of Greenville Police Department and Greenville County Sheriff's Office.
- Work with neighborhood associations to implement Crime Watch programs.
- Implement CPTED (Crime Prevention Through Environmental Design) strategies.

Goal 5: Encourage a balanced transportation network

- Evaluate existing roadway configurations and traffic patterns and work with SCDOT to address safety issues.
- Reduce the number of curb cuts in order to control vehicular access to the corridor.

- Expand sidewalk connectivity in order to establish a pedestrian-friendly environment.

Goal 6: Attract new investment to the area

- Identify and promote key redevelopment opportunities.
- Showcase strengths of the area that include access to major highways and corridors; proximity to downtown, hospitals, schools, etc.; community support and loyalty to the area; and strong neighborhoods with diverse demographics.
- Provide realistic, market-based solutions for economic development initiatives.

Summary of Planning Initiatives

City of Greenville Comprehensive Plan – “Plan-It Greenville”: In 2009, the City of Greenville completed the ten year update to the Comprehensive Plan. This plan, entitled “Plan-It Greenville”, is required by state law and provides a general framework for future development patterns and goals for the entire City, based on input from hundreds of citizens. This plan also includes an implementation strategy which recommended additional neighborhood plans and studies. The Augusta Road Master Plan is just that, a detailed neighborhood plan that identifies specific characteristics of the area and provides recommendations on zoning changes, economic development initiatives, and infrastructure.

City of Greenville Zoning: The majority of the properties located within the city portion of the study area are zoned OD, *Office and Institutional*, or C-2, *Local Commercial*. The OD zoning classification is intended to provide for a wide variety of professional and business offices and institutions. In limited cases, retail uses may be allowed as a special exception and subject to specific criteria if its primary purpose is to serve the workers within the district. Residential uses are also allowed. An OD district may be used as a buffer between residential and nonresidential districts.

The C-2 zoning classification is intended to provide a limited range of general retail, business and service uses as well as professional and business offices, but not intensive business or industrial activities, to person living in surrounding neighborhoods. Residential uses

are also allowed. The C-2 district is typically located on minor arterials and at the intersection of minor and major arterials. In 2010, several properties located within the study area were rezoned from C-3, *Regional Commercial*, to C-2, *Local Commercial*, to make them more compatible with future land use recommendations provided in the City's Comprehensive Plan.

The large majority of the residential properties in the surrounding neighborhoods are zoned R-6, *Single Family Residential*, which is intended to preserve and encourage single-family residential development with a minimum lot size of 6,000 square feet. Complementary uses customarily found in residential districts, such as community recreation facilities, places of worship, and schools may also be allowed in R-6 districts.

Greenville County Comprehensive Plan: In December 2009, Greenville County adopted a new Comprehensive Plan and accompanying future land use map, entitled 'Imagine Greenville County'. The area near Augusta Road and Interstate-85 has been designated a "Regional Center".

Located within a broader area, this Regional Center serves one or more contiguous regions in the County. Residents will typically travel longer distances to these centers on a weekly or biweekly basis. Regional Centers contain large-scale retail such as grocery stores, some big-box stores, small hotels, movie theaters, and medium to large scale employment centers and parks.

Greenville County Zoning: Greenville County designated the majority of the properties located within the county portion of the study area C-2 Commercial, which allows for various commercial and light service uses along major thoroughfares. Establishments in this district provide goods and services for the traveling public and surrounding residential areas.

Note: Greenville County's C-2 Commercial zoning designation is different than the City of Greenville's C-2 zoning designation.

Summary of Community Participation

In January 2011, the City of Greenville retained consultants, Mahan Rykiel Associates and Community Design Solutions, to compile background information

collected by the City into a master plan report. The consultants were also asked to prepare design and graphic products to illustrate design concepts for key sites within the study area. Many of the recommendations provided in this report are the result of a collaborative effort between the consultants, City of Greenville staff and the Augusta Road Steering Committee.

Augusta Road Steering Committee: The Augusta Road Steering Committee was formed in May 2010 to help guide the master planning process and provide valuable feedback to City staff. The committee, made up of residents, business owners and property owners, represented a diverse mix of stakeholders from the study area and adjacent neighborhoods. In order to develop a clear understanding of the planning process, staff provided each committee member with a planning guide containing educational materials and background information. Throughout the planning process, the committee met several times to discuss key issues affecting the study area, including planning and development, zoning and land use, safety, traffic and economic development.

"SWOT Analysis": In May 2010, the steering committee participated in a "SWOT Analysis" to identify the strengths, weaknesses, opportunities and threats of the study area. The following is a summary of key themes developed from their findings:

Strengths:

- Strong neighborhoods with diverse demographics, yet unified sense of community and pride.
- Location, accessibility (gateway), and proximity to downtown, hospitals, schools, etc.
- Existing businesses and neighborhood landmarks such as Pleasant Valley Community Center, Greenville Country Club, Greenville County Library (Ramsey Family Branch), etc.

Weaknesses:

- Jurisdictional issues and lack of coordination among City and County, especially related to crime, safety, zoning/land-use, and overall involvement.

Augusta Road Master Plan

- Lack of effective regulatory tools/policies and plan to guide future development, especially between adjacent commercial and residential properties.
- Property maintenance concerns along the corridor.
 - Large amount of rental housing (lack of homeownership pride), absentee owners and landlords, abandoned/vacant structures, and inadequate code enforcement efforts in neighborhoods.
- Traffic and roadway configuration may be barriers to creating a pedestrian-friendly environment.

Opportunities:

- Ability to attract new investment by showcasing strengths of the area, such as access, location, community support and loyalty, demographics and redevelopment opportunities.
- Numerous redevelopment opportunities, with diverse characteristics, including vacant land, existing structures for renovation, shopping centers, freestanding buildings, sites of various sizes (small neighborhood sites to big box site), etc.
- Opportunity and/or need for infrastructure improvements, improved streetscape, pedestrian connections, etc.

Threats:

- Neighborhood feels unprotected due to lack of control, awareness, and involvement in the development process.
- Lack of momentum, public support and resources to implement plan once it is complete.

Public Open House: In August 2010, the City held a public open house event at the Pleasant Valley Community Center (Figures 1-3). The meeting was well attended with approximately 70 people, representing a mix business owners and residents from the surrounding neighborhoods. The open house, presented in a charrette-style format, provided an opportunity for members of the community to openly discuss the plan with City and County staff, public officials and steering committee members. Following a brief presentation and overview of the master planning

effort, attendees were asked to participate in a visual preference survey to identify aesthetic and design-related preferences for the area. Input collected during the open house was summarized and incorporated into the master plan.



Figure 1: Planning map used during a public open house held in August, 2010.



Figure 2: Community members participate in the planning process during the August, 2010 public open house.



Figure 3: Community members listen to a presentation as part of the August, 2010 public open house.

II: EXISTING CHARACTER

The existing corridor is comprised of a variety of development patterns including large scale commercial, neighborhood-scale office, residential and institutional (Figure 4).



Figure 4:

III: RECOMMENDATIONS

This section of the report highlights the overall master plan (Figure 5) and planning and design concepts to be considered throughout the study area. It includes images of successful precedents that already exist along Augusta Street, general design concepts and the overall illustrative plan.

Successful Precedents along Augusta

Enhancements within the public right-of-way and on private property have already been successful along sections of Augusta Street, especially between Augusta Place and Grove Road. These improvements serve as good precedents for areas included in this Augusta Road Master Plan. Specifically, precedents to note include:

Streetscape Enhancements: Streetscape enhancements include decorative paving standards for sidewalks and the addition of street trees and pedestrian-scaled street lighting (Figure 6).

Buildings Located at Street Edges: Infill development has been effective with buildings near the street edge when feasible (Figure 6).

Outdoor Dining and Gathering Areas: Many businesses have created outdoor gathering areas for dining or relaxing (Figure 7). These outdoor spaces activate the pedestrian environment and promote a sense of community.

Facade Articulation: Well-articulated facades including canopies and awnings provide visual interest (Figure 8).

Parking Lot Landscaping: Landscaping within the parking areas is effective at breaking up large expanses of pavement and providing shade and visual interest (Figure 8). In addition, large canopy trees provide shade and reduce the overall scale of paved areas while allowing views to buildings and sign panels (Figure 9).



Figure 6



Figure 7



Figure 8



Figure 9



Figure 5 Illustrative Master Plan

Overall Design Concepts

Major Gateways: Major gateways to the study area occur at the three interchanges along I-85: Augusta Road, Pleasantburg Drive and Mauldin Road.

- **Landscape Treatment:** The landscape treatment at the major gateways should reflect bold simple masses of shrubs and large trees appropriate to the scale of the interchange (Figures 10-13).
- **Gateway Signs:** Provide “Welcome to Greenville” signs at each major gateway interchange. The design should be monumental (Figure 13) and compatible with the existing Greenville City limit signs and other “Welcome to Greenville” signs (Figure 14).

Neighborhood Gateway: The primary “neighborhood gateway” to the Augusta Road corridor occurs at the intersection of Augusta and Mauldin Roads. Following are some key components of this gateway. This intersection is also described and illustrated in detail on page 23-26 of this master plan.

- **Gateway Signs:** Provide neighborhood gateway signs with “Only on Augusta” or a similar message highlighting the Augusta Road neighborhood.
- **Landscape Treatment:** The landscape treatment for this gateway should also reflect a simple bold landscape palette of canopy trees and shrubs to make the most impact. Due to constraints within the public right-of-way, much of this landscape will occur within adjacent private properties.



Figure 10



Figure 11



Figure 12

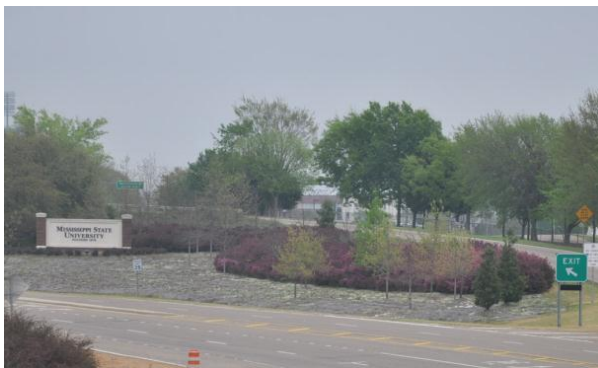


Figure 13



Figure 14



Streetscape: The following design standards are recommended throughout the study area. Many of these standards are currently being used to the north along Augusta Street.

- **Consolidation of Curb Cuts:** Redundant curb cuts should be closed or consolidated to better delineate traffic flow, minimize conflicts between vehicles and pedestrians and enhance the pedestrian environment.
- **Center Median Lanes:** The main roadways within the study area (Augusta Road, Mauldin Road and Pleasantburg Drive) are often characterized by a center median lane that also accommodates turning vehicles in both directions. The result is 5 or more lanes of uninterrupted asphalt. Consideration should be given to exploring with SCDOT the use of stamped colored asphalt within these median lanes to provide visual interest and break up the expanse of pavement (Figure 15).
- **Raised Medians:** Some roadways within the study area, including Augusta and Mauldin Roads, utilize raised concrete medians. Consideration should be given to utilizing brick, concrete pavers or stamped colored concrete within these medians to allow them to be more prominent and to visually break up broad expanses of pavement (Figure 16).
- **Curb Lawns:** Curb lawns should be utilized where possible to provide separation between the sidewalk and roadway and to provide a planting strip for street trees. Curb lawns should adhere to the following dimensions:
 - 6' wide or greater when planted with canopy trees,
 - 4' wide when planted with ornamental trees, and
 - 2' wide when planted in lawn only.
- **Sidewalk Treatments:** Sidewalks should be added where none exist and enhanced to reflect the design character already established along Augusta Street using scored concrete with brick banding (Figure 17). Sidewalks should be a minimum of 5' in width and should be designed to meet all ADA requirements. In areas of heavy commercial activity, sidewalk widths should be greater if space allows.



Figure 15



Figure 16

- **Crosswalk Treatments:** Wide crosswalks should be provided at all intersections to visually distinguish the pedestrian crossing zone. Crosswalks should be a minimum of 10' wide at major intersections (such as Augusta and Mauldin) and a minimum of 8' wide at smaller intersections. Crosswalk materials should utilize white thermoplastic paint borders and stamped asphalt in a color and pattern that will complement the brick used within the sidewalk zones (Figure 18).
- **ADA Sidewalk Ramps:** Ramps should be provided at transitions between crosswalks and sidewalks at roadway intersections and curb cuts. The ramps should utilize colored and textured pavers at the landing zones. Because ADA regulations change periodically, current requirements should be be verified for ramp design and surface treatment.
- **Pedestrian Plazas:** Where possible, pedestrian plazas should be provided at key intersections. These plazas can simply include expanded sidewalk areas to signify areas where pedestrian flows intersect or larger gathering areas associated with new commercial building development. The design of each area should reflect the unique character of the space, but also complement the design standards used for the sidewalks. The plazas may include site furniture and amenities, decorative paving, street lighting, public art and distinctive landscaping (Figure 19).

Additional precedent images illustrate how small pedestrian plazas can be incorporated and how stamped asphalt and stamped concrete can be used effectively (Figure 20).



Figure 17



Figure 18



Figure 19

Augusta Road Master Plan

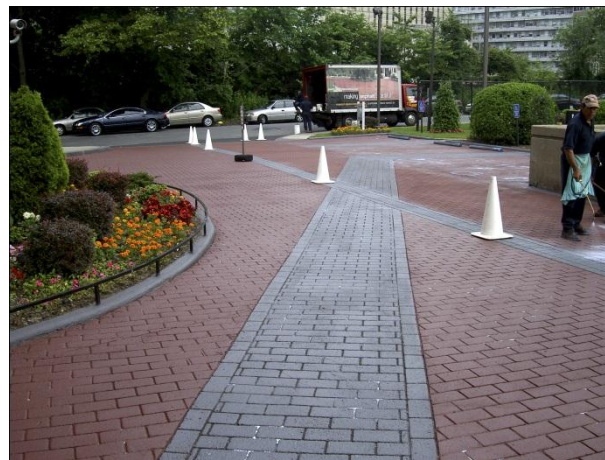


Figure 20

- **Street Furniture and Amenities:** Street furniture and amenities include benches, trash receptacles, bike racks and planters and help to reinforce a pedestrian environment. While the street furniture and amenities for areas immediately adjacent to buildings should reflect the character of that building or business, amenities that occur as part of the streetscape should utilize consistent standards as identified and shown below (Figure 21):

Bench: Victor Stanley, Steelsites Series, Black

Trash Receptacle: Victor Stanley, Ironsites Series, Black

Planter: Victor Stanley, Ironsites Series, Black

- **Decorative Street Signs:** Provide decorative street signs along Augusta Street throughout the study area. The initial phase should occur between the Blythe Academy Intersection Area and Lydia Street (Figure 22).
- **Pedestrian-Scaled Street Lights:** Decorative street lights provide a uniform aesthetic character to the streetscape and enhance pedestrian safety. The Duke Energy Deluxe Acorn is already being used along Augusta Street and is the standard recommended for use throughout the study area (Figure 23).
- **Roadway Lights:** In addition to pedestrian-scaled lights, decorative street/roadway lights should be used throughout the study area, particularly at intersections. The standard roadway light recommended for use is the Duke Energy Boston Harbor Teardrop (Figure 24).
- **Mast Arms:** A significant amount of visual clutter can be minimized by utilizing signal mast arms at intersections. In addition to accommodating the signal lights, these mast arms would accommodate signal mechanisms for pedestrians, street lights and street signs. The actual specification should be compatible in design and color as the street lights.



Bench



Planter



Trash Receptacle

Figure 21



Figure 22



Figure 23



Figure 24

- Street Trees:** In addition to their aesthetic qualities in enhancing the streetscape and adjacent private properties, street trees provide pedestrian comfort and help to calm traffic. The most effective street trees are tall canopy trees that visually reduce the roadway expanse while providing shade. In addition, tall canopy trees do not obscure buildings or signs as they allow for views beneath their canopies. Mature ornamental trees, on the other hand, often obscure views to buildings and do little to provide shade or reduce the scale of the roadway. Still, the presence of overhead utilities will require the use of small ornamental trees along the street edge. If this is the case, single stem varieties are encouraged vs. dense “shrubby” varieties with multiple stems.

The City and County should work with private property owners to provide tall canopy trees within their property if there is inadequate space along the street. These may occur in setback areas or within parking lot islands.

The following canopy shade trees and ornamental trees (Figure 25) should be considered but not limited to:

- Overcup Oak
- Nuttall Oak
- Shumard Oak
- Willow Oak
- White Oak (large lawn areas only)
- Japanese Zelkova
- Okame Cherry (single trunk when used as street tree)
- Crape Myrtle (single trunk when used as street tree)



Overcup Oak



Nuttall Oak



White Oak



Shumard Oak



Japanese Zelkova



Crape Myrtle



Okame Cherry (single and multi-stem)



Willow Oak

Figure 25

- Public Art:** Public art should be incorporated into the streetscape environment, particularly at major gateways and areas with frequent pedestrian traffic and gathering. The use of local artists is encouraged. Art should reflect the scale of the environment within which it is placed. For example, highway-oriented or large intersections should feature significant art displays that can be experienced from the automobile. Similarly, smaller pedestrian spaces should feature smaller art pieces, appropriate to the scale of the space (Figure 22).



Source: Georgia Gerber



Source: Georgia Gerber

Figure 22

IV: AUGUSTA-MAULDIN GATEWAY

This portion of the master plan describes and illustrates recommendations for public and private property within and adjacent to the intersection of Augusta and Mauldin Roads. These recommendations also serve as precedents for other intersections and properties within the study area. This gateway area includes the intersection itself, Goodwill Shopping Center, Pickwick Center and Potomac Square (Figure 27).

Intersection Enhancements

Existing Conditions: The existing intersection is vast in scale but has been improved significantly with the addition of planting islands and landscaping at each corner and in some of the median areas. As the trees in the islands mature, they will continue to enhance the overall appearance of this intersection gateway (Figure 28).

Some of the existing conditions, however, continue to keep the intersection from being a pedestrian-friendly and attractive gateway. Crosswalks are not very visible and there remains a large expanse of asphalt throughout the intersection. In addition, overhead signal wires are unattractive and the street corners are not firmly anchored with architecture and/or canopy shade trees.

Recommendations: The following recommendations will help to reinforce this intersection as the gateway to the Augusta Road neighborhood and create a more pedestrian-friendly environment. When completed, it can serve as a model for other large intersections within the study area (Figures 29-31).

- **Raised Medians:** Consider utilizing brick, concrete pavers or stamped colored concrete within the narrow raised medians near the intersection to visually distinguish them from the roadway pavement and to add aesthetic interest to the intersection approach.
- **Center Median Lanes:** Consider stamped colored asphalt within the center turn lanes.
- **Crosswalk Treatment:** Utilize stamped colored asphalt for the crosswalks.
- **Sidewalk Treatment:** Utilize standard aesthetic treatment for sidewalks within the study area (scored concrete and brick banding).
- **Intersection Pavement:** Utilize stamped colored asphalt in the center of the intersection in a color complementary to the crosswalk color.
- **Mast Arms:** Provide mast arms for traffic signals. Prepare a detail design of the intersection to determine most appropriate quantity, length and placement of mast arms.
- **Roadway Lighting:** Provide decorative roadway lighting along Augusta Road, Augusta Street and Mauldin Road.
- **Pedestrian-Scaled Street Lighting:** Supplement roadway lighting with decorative pedestrian-scaled lighting.
- **Public Art:** Work with private property owners and the City's Arts In Public Places Commission to provide public art near intersection corners, particularly in areas where crosswalks and sidewalks converge (pedestrian gathering areas).
- **Gateway Signs:** Provide City of Greenville gateway signs along Augusta and Mauldin Roads.
- **Landscape Treatment:** Provide additional street trees where possible. Work with adjacent property owners to add canopy trees and hedges within their properties, particularly along roadway sections where street trees cannot be planted.



Figure 27: Augusta-Mauldin Gateway Area

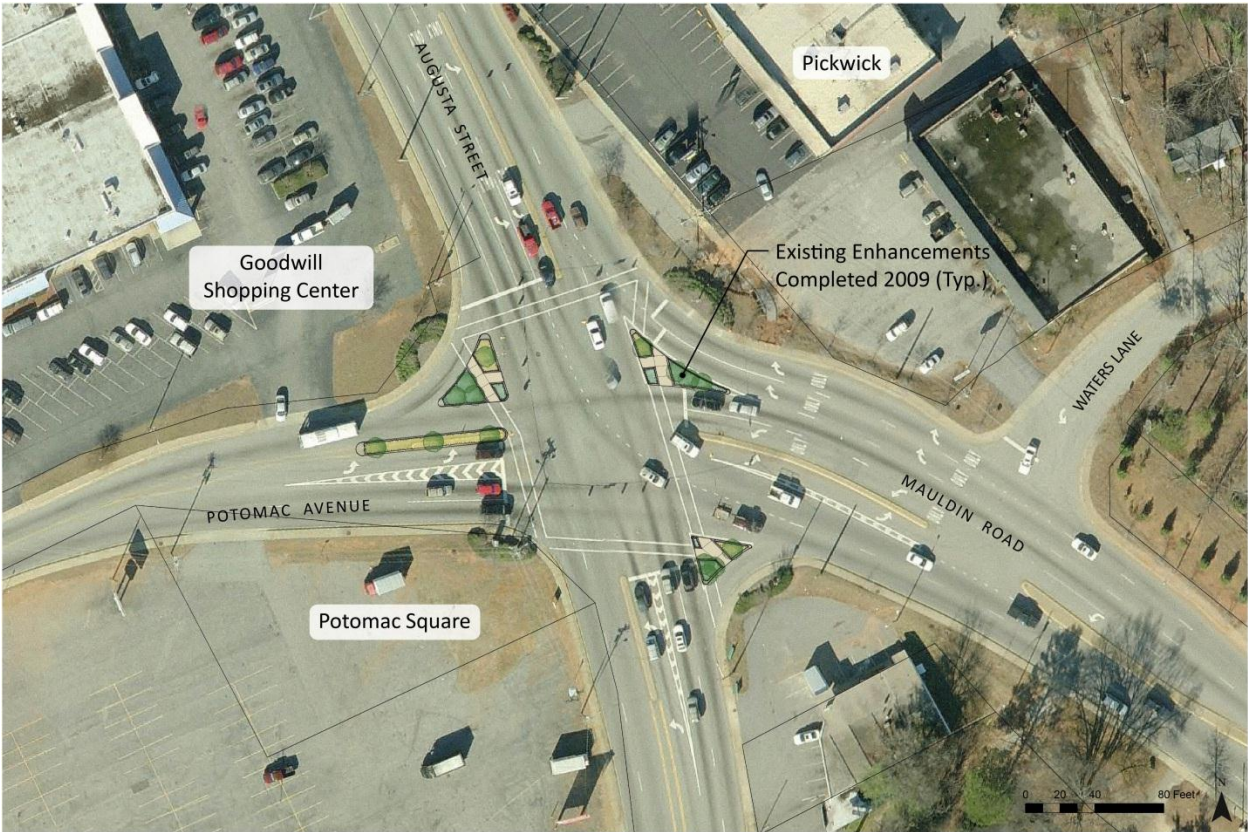


Figure 28: Augusta-Mauldin Gateway Existing Intersection

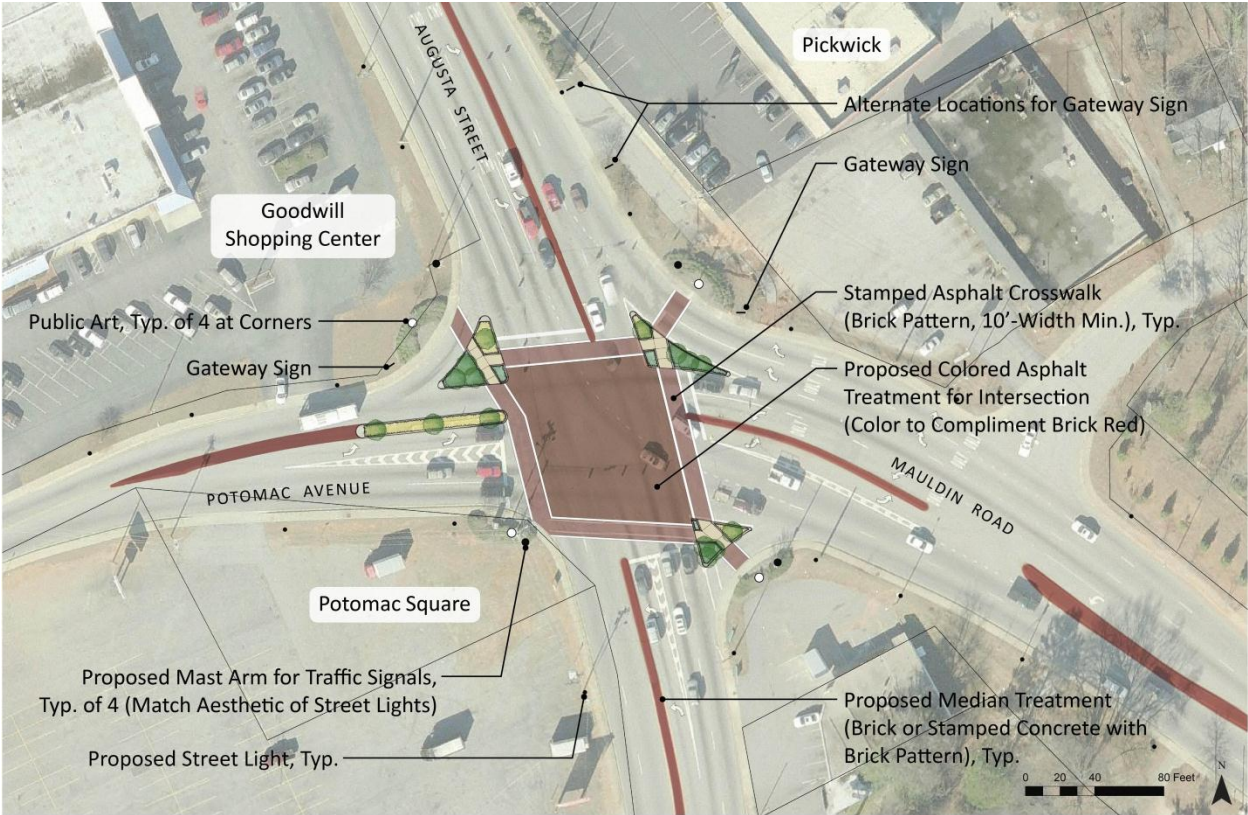


Figure 29: Augusta-Mauldin Gateway Proposed Enhancements



Figure 30: Augusta-Mauldin Gateway Intersection Existing and Interim Potential View



Figure 31: Augusta-Mauldin Gateway Intersection Potential View

Goodwill Shopping Center

The shopping center at the northwest corner of the intersection of Augusta Street and Mauldin Road is anchored by Goodwill and the popular local restaurant, Sarah's Kitchen. The center currently does not have a name. For the purposes of this master plan, it will be referred to as the "Goodwill Shopping Center."

Existing Conditions: Due to its prime location and visibility, the Goodwill Shopping Center has the potential to be an attractive shopping destination along Augusta Street. Currently, however, the center appears to be rather stark and exposed, as illustrated in Figures 32-35. Specific observations include:

- Bright white facade and light blue awnings result in significant glare and lack of warmth (Figure 32).
- Southern exposure and lack of canopy trees accentuates heat and glare coming from pavement (Figures 32-34).
- Landscape areas along Augusta Street and Potomac Avenue are barren but provide opportunities for more efficient parking organization and addition of shade trees and low accent planting (Figure 33).
- Small ornamental trees are out of scale with the road and parking area and should be supplemented with canopy trees (Figure 30).
- Sarah's Kitchen is a popular dining destination but lacks an inviting entrance facade or outdoor dining area (Figure 34).
- There is excessive pavement within the parking area particularly near the corner, resulting in inefficient parking (Figure 34).



Figure 32



Figure 33



Figure 34

Recommendations: The following should be considered by the property owner as a way to enhance the shopping center and to reposition it as an important anchor within neighborhood (Figures 36-38).

- **Name:** The property owners should consider an appropriate name and “re-brand” the center with new signage.
- **Parking Organization:** Reorganize parking along Augusta Street to reduce the drive aisle to 24’ in width and create parallel parking along the Augusta Street setback (in place of angled parking and excessive drive width). This will allow room for a landscape strip/hedge between the sidewalk and parking.

Explore the feasibility of reorganizing the parking along Potomac Avenue to create perpendicular parking rather than angled parking. This may require an easement encroachment of about 2’ (and coordination with the City and SCDOT) on the western most portion of the setback area, but will result in a more efficient layout and the ability to introduce additional landscape/tree islands.

Extend parking bays along Augusta Street to the south to capture expansive paved area for landscape islands and additional parking spaces.

The parking reorganization results in a net gain of approximately 23 parking spaces.

- **Service Area Reorganization:** Reorganize the service and loading area on the west side of the building to capture additional parking spaces. Relocate the Goodwill drop-off trailer and screen the service and loading area with landscape.
- **Landscape:** Provide low hedge along Augusta Street to screen parked cars and help unify the streetscape. Utilize tall canopy trees in tree islands and replace the Crape Myrtles to provide more shade while allowing for views to the sign panels and storefronts.
- **Outdoor Dining Terrace:** Consider terrace for Sarah’s Kitchen along the south side of the building to provide opportunities for outdoor dining. Provide colorful umbrella tables to energize blank facade and highlight the restaurant.

- **Public Art:** Consider providing an easement at the corner of the intersection to allow for public art as described under the intersection enhancements on the previous pages. Also consider a mural on the blank wall (south facade) of Sarah’s Kitchen.
- **Facade Enhancements:** Consider painting facade and metal awnings in a warmer color palette. Replace existing freestanding signs with monument signs more consistent with the scale of the center.



Figure 35: Goodwill Shopping Center Existing Conditions



Figure 36: Goodwill Shopping Center Proposed Enhancements



Figure 37: Goodwill Shopping Center Existing and Interim Potential View



Figure 38: Goodwill Shopping Center Potential View

Pickwick Shopping Center

The Pickwick Shopping Center is a neighborhood retail center located at the northeast corner of the intersection of Augusta and Mauldin Roads. The center is a prominent visual and community landmark, and includes the Pickwick Pharmacy and Soda Fountain.

Existing Conditions: The center includes the Pickwick Pharmacy as the anchor tenant along with several other tenants. The building adjacent to the site includes a pawn shop and vacant night club. The Pickwick Pharmacy is an important meeting spot for the community. The building is aesthetically pleasing with its awnings and canopy. The center could be improved with enhancements to the site and minor enhancements to the building (Figures 39-42).

- The center has prominent visibility at this gateway site. However, the existing Cape Myrtles planted within the right-of-way are low-canopied trees and obscure views to the building and signage (Figure 39).
- As a result of the roadway improvements to the intersection, a roadway spur remains, linking the pawn shop/vacant night club building (Figure 40) to the right-in/right-out access onto Augusta Street. This spur is redundant, however, and results in excessive pavement. Additionally, the existing right-in/right-out drive is somewhat redundant as access into and out of the site is already accommodated further to the north on Augusta Street and onto Mauldin Road.

In addition, the existing parking lot is wider than the 60' necessary to accommodate perpendicular parking in front of the Pickwick building. A lack of striping/organization in front of the pawn shop results in inefficient use of the lot.

- This corner of the intersection offers substantial landscape area and is not fully utilized (Figure 41).
- There is a generous sidewalk width adjacent to Pickwick's Soda Fountain and adjacent Little Caesars.
- The lack of canopy trees results in limited shade for the site, particularly when considering the southwestern exposure.

- The existing primary building (Pickwick) is quite attractive with brick facade and simple canopy structure, revealing an overall Art Deco character. The existing sign letters for Pickwick Pharmacy that overhang the roofline detract from the simplicity of the architecture, however.



Figure 39



Figure 40



Figure 41

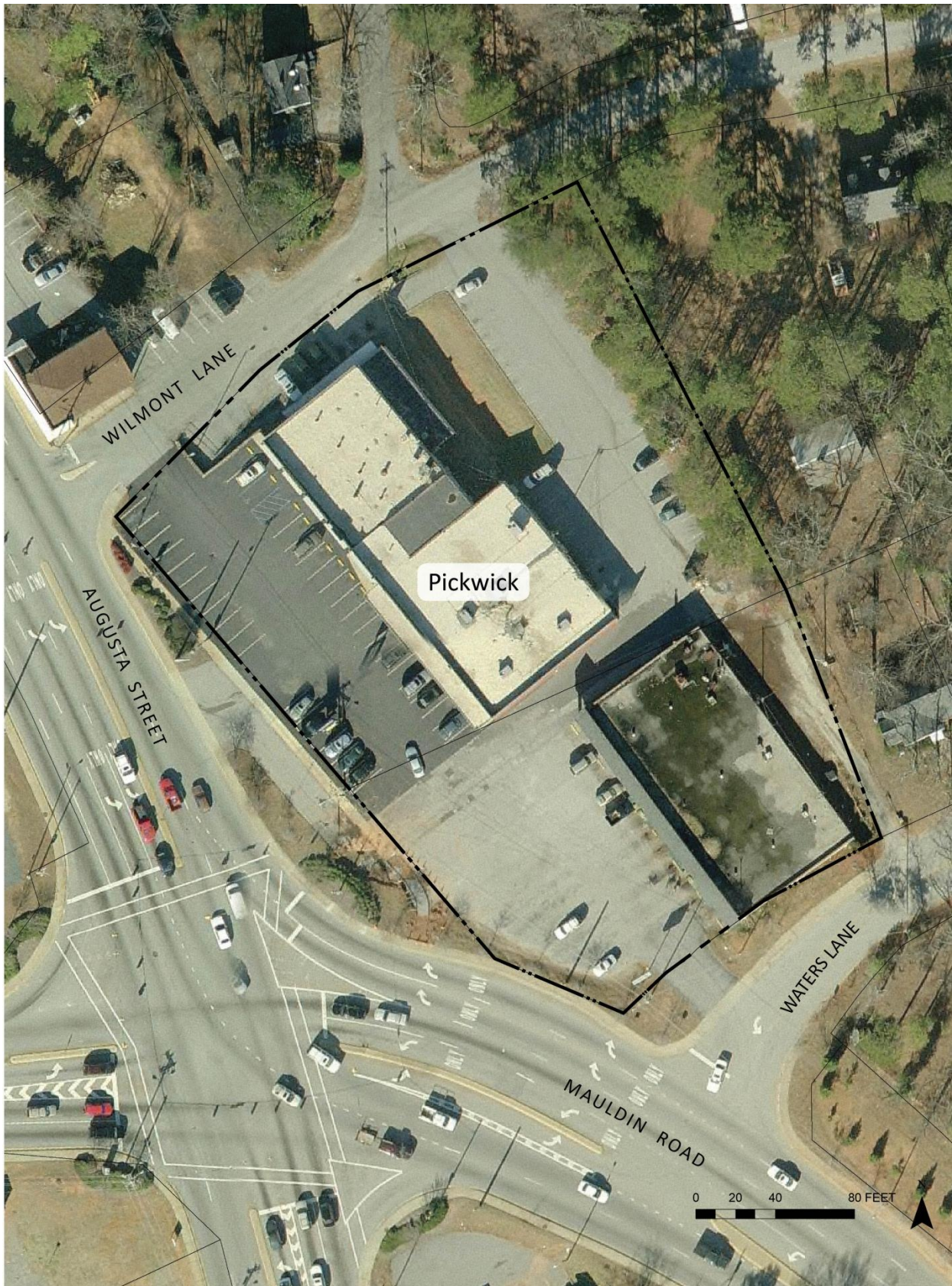


Figure 42: Pickwick Shopping Center-Existing Conditions

Recommendations: Recommendations for enhancements to the Pickwick Shopping Center and adjacent property include (Figures 43-47):

the metal barriers from doors and windows to create a “friendly” storefront and avoid the perception that the area is unsafe.

- **Parking Organization:** Narrow the parking bay in front of the Pickwick Pharmacy to 60’ to capture additional sidewalk area in front of the building for outdoor dining and special accent planting.
- **Signs:** Replace existing free-standing sign with monument sign that is more consistent with the neighborhood-scaled development patterns.

Organize the parking with striping and landscape/tree islands in front of the pawn shop building, located on the property adjacent to the Pickwick Shopping Center.

- **Pavement Reduction:** Eliminate the spur road and incorporate planting islands within the parking lot. Because of the southwestern exposure, trees in the planting islands closest to the building will provide shade outdoor dining and gathering areas. While the property owner desires to keep the access drive open, consider working with property owner to evaluate impacts if it were to be closed in the future. Orange cones could be used to temporarily “close” it for a period of time to evaluate impacts without making an investment. The closure would allow for 2-3 additional parking spaces.
- **Outdoor Dining:** Utilize the wide sidewalk in front of the Pickwick Soda Fountain and Little Caesars to provide umbrella tables for outdoor dining.
- **Landscape:** Provide canopy trees within the landscape islands, particularly close to the sidewalk in front of the buildings to create comfortable outdoor gathering areas. Replace some or all of the Crape Myrtles with large canopy trees adjacent to Mauldin Road and Augusta Street. This will help to reinforce this area as a gateway and will also help to visually reduce the overall scale while allowing for visibility to the storefronts underneath the canopies.

Provide a hedge along the parking areas that face Augusta Street and Mauldin Road to screen parking and visually reinforce the sidewalk area.

- **Facade Enhancements:** Replace existing “Pickwick Pharmacy” sign with block letters that match those inside the café area. Contain the letters completely within the sign panel portion of the facade for a clean appearance. Consider removing



Figure 43: Pickwick Shopping Center-Proposed Enhancements



Figure 44: Pickwick Shopping Center Existing View



Figure 45: Pickwick Shopping Center Potential View



Figure 46: Pickwick Shopping Center Existing View



Figure 47: Pickwick Shopping Center Potential View

Potomac Square

The Potomac Square site at the southwestern corner of the intersection of Augusta and Mauldin Roads presents a significant opportunity for redevelopment.

Existing Conditions: The existing building sets approximately 15-20' lower than the intersection of Augusta and Mauldin Roads and is separated from the roadways by a large field of parking. This results in a weak definition of the gateway intersection. This grade also creates challenges for redevelopment, particularly when there is a desire to construct new buildings at the street edge, with parking located to the rear. The furniture store building to the south is also available for purchase, so this site should be taken into consideration for redevelopment in conjunction with the Potomac Square site (Figures 48-51).

The area quickly transitions from a commercial to residential area along Potomac Avenue (Figure 50).

- Sewer Conditions/Capacity:** The sewer capacity for Potomac Square was assessed by the City's Stormwater Engineer in September, 2010. Both the City and Gantt have sewer service adjacent to the parcel. The parcel is currently served by Gantt Sewer (Special Purpose District) due to the pitch of the site.

Gantt reported no capacity issues based on the current use and could supply flow to a similar type of use with the same square footage. The private main that serves the parcel prior to connecting to Gantt's main is in poor condition. If the site is redeveloped and the sewer connected to Gantt, then the private main would need to be replaced.

As for connecting to the City's sewer main located on Potomac Avenue, the City currently has enough capacity to accommodate an additional 42,000 square feet of development (excluding restaurants). In order to connect to the City's sewer, the building would need to be elevated to at least 3 feet above the crown of Potomac (which results in a good bit of fill on site) and the building placed directly adjacent to Potomac.



Figure 48



Figure 49



Figure 50



Figure 51: Potomac Square Existing Conditions

Recommendations: While the Potomac Square site would lend itself well to a pedestrian-scaled mixed-use development, the economic realities for the site indicate that redevelopment should be geared toward a large floorplate national retailer. Consideration should, however, be given to providing some smaller-scaled development at the corner of Augusta and Mauldin Roads. This would be particularly appropriate because of Potomac Avenue's transition into a residential area.

The site plan on the following pages illustrates how the site could be redeveloped (Figures 52-60). Specific recommendations include:

- **Site Grading:** Because the site grades fall away from Potomac Avenue, the southern portion of the site can be cut to the lower elevation with the fill used to create a "platform" of parking and development along Potomac Avenue. For purposes of this master plan, it is assumed that an excessive amount of fill from off-site will not be brought in.
- **Building Locations:** Locate large floorplate building(s) toward the southwest corner of the site to face Augusta Road. This will allow the side facade to be oriented toward Potomac Avenue and the front oriented toward Augusta Road. The rear facade should face west and be screened from the adjacent residential neighborhood.

Locate smaller buildings near the corner of Augusta and Mauldin Roads. Orient smaller buildings close to the street to reinforce the pedestrian environment and help anchor the intersection (Figure 52).

- **Building Uses:** The smaller buildings near the street could be developed as retail, restaurant and/or office uses (Figures 52 and 53).
- **Architectural Design:** Utilize a high degree of facade articulation for all buildings, particularly for large floorplate buildings (Figures 52-56).
- **Pedestrian Plazas:** Pedestrian plazas should be provided, particularly adjacent to the smaller buildings near the street edge. There is an opportunity to create a prominent entry plaza linking the street intersection with the internal portions of the site. This space could be activated

by outdoor dining, seating and accent plantings (Figure 53).

- **Streetscape:** Tall canopy shade trees should be used along Potomac Avenue and Augusta Road in areas where there are no conflicts with overhead utilities to create a strong street edge (Figure 59). The upper parking area along Potomac Avenue should be screened with a hedge which will work well to effectively screen the parking which may be lower than the street level. Surface parking along Augusta Road should also be screened with a hedge and landscaping. Additional streetscape amenities as described elsewhere in this master plan should be used along both street edges to create attractive pedestrian environments.
- **Site Landscape Treatment:** Additional landscape should be provided throughout the site in accordance to the zoning ordinance. Special emphasis should be placed on landscape treatments for pedestrian plazas to provide shade and seasonal interest.
- **Public Art:** The property owner should work with the City to provide public art near the corner of Augusta and Mauldin Roads.



Figure 52



Figure 53



Figure 56



Figure 54



Figure 57



Figure 58



Figure 55



Figure 59



Figure 60: Potomac Square Proposed Enhancements

V: BLYTHE ACADEMY AREA

This portion of the master plan outlines and illustrates recommendations for enhancements to the intersection of Augusta Street and Blythe Drive/Riverside Drive and the small retail centers located on the southwest corner of this intersection. The streetscape enhancements described and illustrated below should serve as a prototype for Augusta Street throughout the study area.

Streetscape Enhancements

The recommendations for the Augusta Street streetscape are essentially to continue the treatment that has been used effectively further to the north, using the design guidelines described in this report.

Existing Conditions: The area includes a variety of conditions, including a mix of architectural styles and scales of commercial, residential and institutional buildings. As is typical with older commercial corridors, this streetscape is characterized by numerous, and often redundant, curb cuts, few street trees, inadequate sidewalks, utilitarian street lighting, overhead utilities, poorly marked crosswalks and some outdated building facades (Figures 61-64).

Recommendations: Following are some key recommendations for streetscape enhancements. Many of these recommendations are for elements that occur within the public right-of-way while others are potential enhancements to private properties (Figures 65-67).

- **Consolidated Curb Cuts:** Minimize redundancy of access drives, enhance pedestrian and landscape zones and improve safety by consolidating curb cuts.
- **Crosswalk Treatment:** Utilize colored stamped asphalt paving within crosswalk areas, as described in this master plan, to visually distinguish them and create strong visual cues to motorists that Augusta Street is pedestrian-friendly.
- **Sidewalk Treatment:** Enhance and repair sidewalks and utilize the paving standards outlined in this master plan.



Figure 61



Figure 62



Figure 63



Figure 64

Augusta Road Master Plan

- **Pedestrian Plazas:** Where possible, provide expanded sidewalk areas/pedestrian plazas at key intersections, such as the intersection of Augusta Street and Blythe Drive, to accommodate pedestrian connections, site furniture and amenities, particularly along school routes.
- **Mast Arms:** Provide mast arms to accommodate signals, street lights and street signs at intersections and reduce overhead wires.
- **Street Lights:** Utilize pedestrian-scaled street light standards and roadway light standards along the entire corridor.
- **Public Art:** Partner with the City's Arts in Public Places Commission to incorporate public art within pedestrian gathering areas such as the intersection of Augusta Street and Blythe Drive (Kiwanis Park).
- Consider bronze sculptures such as those by Georgia Gerber (Portland, OR) or others, particularly those by local artists.
- **Landscape Treatment:** Where there are no conflicts with overhead utilities, utilize tall canopy trees to provide shade and reduce the scale of the street, while allowing views to commercial buildings and signage.

Where landscape strips exist between the sidewalk and surface parking lots, provide low hedges to soften the edge of the parking lot and create a more attractive pedestrian experience and unified landscape theme along the corridor.

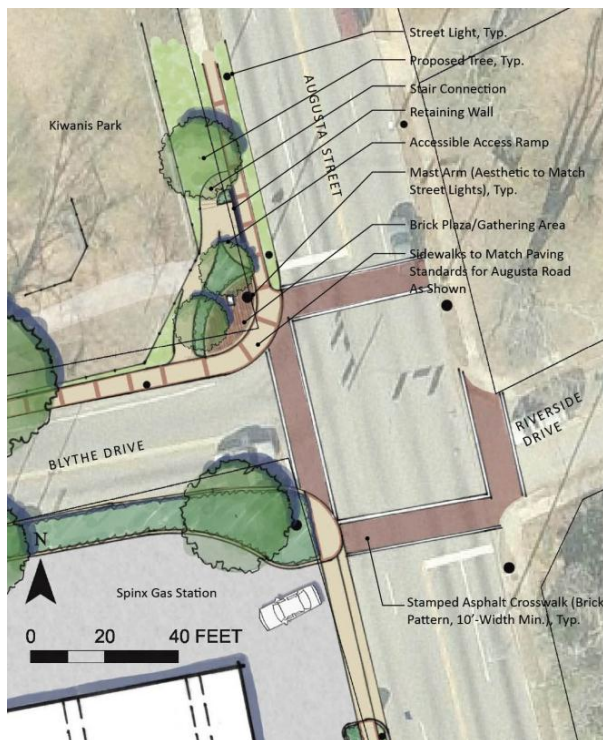


Figure 65: Augusta Street Streetscape Proposed Conditions



Figure 66: Augusta Street Streetscape Existing and Interim Potential View



Figure 67: Augusta Street Streetscape Long-Term Potential View

Riverside Center

The existing retail center located south of the intersection of Augusta Street and Blythe Drive/Riverside Drive includes several businesses and a Spinx service station. While the center does not have a name, for the purposes of this master plan, it is referred to as “Riverside Center”.

Existing Conditions: The existing center is at a prominent location along Augusta Street. Because of its manageable size, the center presents an opportunity to incorporate enhancements to the site and building facades and establish a more positive image for the area (Figures 68-72).

Existing site conditions include:

- Excessive nonconforming signs.
- Lack of landscape strip between parking area and sidewalk and lack of internal site landscape.
- Excessive parking bay width (Figure 68).
- Weak facade treatments (Figures 68 and 69).
- Excessive curb cuts along Augusta Street and Old Augusta Road, resulting in disconnected sidewalk and multiple conflict points between vehicles and pedestrians (Figure 70).
- Underutilized alley /courtyard behind the existing retail buildings (Figure 71).



Figure 68



Figure 69



Figure 70



Figure 71



Figure 72

Recommendations: The following recommendations represent potential enhancements that the property owners could make to improve the overall image of the center and make the center more pedestrian-friendly. The City should work with the Spinx service station to redevelop the site with a new building and site layout (Figure 73). The enhancements described below could be completed in conjunction with this redevelopment (Figures 74-78).

- **Pavement Reduction:** Consolidate curb cuts and better utilize the side and rear of the property to improve circulation and enhance pedestrian safety. In particular, eliminate the curb cut into Spinx that is located closest to the street intersection.

Parking Organization: Narrow the parking bay in front of the retail buildings to 60' to allow the creation of a planting strip along Augusta Street. Provide internal landscape/tree islands to divide the parking into smaller areas.

- **Landscape Treatment:** Provide a low hedge along the sidewalk to better define the pedestrian environment. Provide canopy trees within landscape islands to provide shade and reduce the overall scale of the parking area.
- **Outdoor Dining Areas:** Utilize the alley space between buildings to take advantage of the strong spatial definition provided by the adjacent walls. Utilize this area for outdoor dining and consider the use of colorful umbrellas to animate the space. In addition, close the access drive at the southern end of the center to take advantage of this area for additional outdoor seating.
- **Public Art:** Utilize blank walls for murals adjacent to the outdoor dining areas.
- **Lighting:** Utilize string lights to accent the outdoor dining area for evening use.
- **Facade Enhancements:** Utilize contemporary awning structures and sign panels to enhance the humble building facades and provide new windows with light color mullions to make the facade more inviting.

- **Name:** Consider giving the center a name and rebranding as a neighborhood commercial center. Names could include "Riverside Center" "Blythe at Augusta", etc.



Figure 73



Figure 74: Proposed Riverside Shopping Center and Spinx Service Station.



Figure 75: Riverside Center Existing and Interim Potential View



Figure 76: Riverside Center Potential View



Figure 77: Riverside Center Existing View



Figure 78: Riverside Center Potential View

